



SAN FRANCISCO
PLANNING
DEPARTMENT

CEQA Categorical Exemption Determination

Property Information/Project Description

PROJECT ADDRESS <i>Pier 84 - Copra Row</i>	BLOCK/LOT(S) <i>9900 / 084</i>
CASE NO. <i>2013.0447 E</i>	PERMIT NO.
	PLANS DATED <i>5/5/03</i>

- Addition/ Alteration (detailed below)
 Demolition (requires HRER if over 50 years old)
 New Construction

STEP 1 EXEMPTION CLASS

- Class 1: Existing Facilities**
 Interior and exterior alterations; additions under 10,000 sq.ft.; change of use if principally permitted or with a CU.
- Class 3: New Construction**
 Up to three (3) single family residences; six (6) dwelling units in one building; commercial/office structures under 10,000 sq.ft.; accessory structures; utility extensions.

CLASS 31 - Historical Resource Restoration/Rehabilitate

NOTE:
If neither class applies, an *Environmental Evaluation Application* is required.

STEP 2 CEQA IMPACTS (To be completed by Project Planner)

If ANY box is initialed below an *Environmental Evaluation Application* is required.

- _____ **Transportation:** Does the project create six (6) or more net new parking spaces or residential units? Does the project have the potential to adversely affect transit, pedestrian and/or bicycle safety (hazards) or the adequacy of nearby transit, pedestrian and/or bicycle facilities?
- _____ **Air Quality:** Would the project add new sensitive receptors (specifically, schools, colleges, universities, day care facilities, hospitals, residential dwellings [subject to Article 38 of the Health Code], and senior-care facilities)?
- _____ **Hazardous Materials:** Would the project involve 1) change of use (including tenant improvements) and/or 2) soil disturbance; on a site with a former gas station, auto repair, dry cleaners, or heavy manufacturing use, or on a site with underground storage tanks?
Phase I Environmental Site Assessment required for CEQA clearance (E.P. initials required)
- _____ **Soil Disturbance/Modification:** Would the project result in the soil disturbance/modification greater than two (2) feet below grade in an archeological sensitive area or eight (8) feet in non-archeological sensitive areas?
Refer to: EP ArcMap > CEQA CatEx Determination Layers > Archeological Sensitive Areas
- _____ **Noise:** Does the project include new noise-sensitive receptors (schools, colleges, universities, day care facilities, hospitals, residential dwellings, and senior-care facilities) fronting roadways located in the noise mitigation area?
Refer to: EP ArcMap > CEQA CatEx Determination Layers > Noise Mitigation Area
- _____ **Subdivision/Lot-Line Adjustment:** Does the project site involve a subdivision or lot-line adjustment on a lot with a slope of 20% or more?
Refer to: EP ArcMap > CEQA CatEx Determination Layers > Topography

Slope = or > 20%: Does the project involve excavation, square footage expansion, shoring, underpinning, retaining wall work, grading – including excavation or fill?

Exceptions: Do not check box for work performed on previously graded level portion of site; stairs, patio, deck and fence work.

Geotechnical report required and a Certificate or higher level CEQA document required – File an Environmental Application

Seismic: Landslide Zone: Does the project involve excavation, square footage expansion, shoring, underpinning, retaining wall work, grading – including excavation and fill on a landslide zone – as identified in the San Francisco General Plan?

Exceptions: Do not check box for stairs, patio, deck and fence work.

Geotechnical report required and a Certificate or higher level CEQA document required – File an Environmental Application

Seismic: Liquefaction Zone: Does the project involve excavation, square footage expansion, shoring, underpinning, retaining wall work, grading – including excavation and fill on either seismic, flooding, or liquefaction zone?

Exceptions: Do not check box for stairs, patio, deck and fence work.

Geotechnical report will likely be required. File an Environmental Application

Serpentine Rock: Does the project involve any excavation in a property containing serpentine rock?

No exceptions.

File an Environmental Application to determine the applicable level of CEQA analysis

NOTE:

Project Planner must initial box below before proceeding to Step 3.

Project Can Proceed With Categorical Exemption Review.

The project does not trigger any of the CEQA Impacts and can proceed with categorical exemption review.

GO TO STEP 3

*MP
104/16/13*

The Port will standard follow the standard used in similar projects that are cleared under the maintenance & Repair Code.

STEP 3 PROPERTY STATUS - HISTORICAL RESOURCE

Property is one of the following: (Refer to: San Francisco Property Information Map)

Category A: Known Historical Resource **GO TO STEP 5**

Category B: Potential Historical Resource (over 50 years of age) **GO TO STEP 4**

Category C: Not a Historical Resource or Not Age Eligible (under 50 years of age) **GO TO STEP 6**

STEP 4 PROPOSED WORK CHECKLIST (To be completed by Project Planner)

If condition applies, please initial.

1. **Change of Use and New Construction** (tenant improvements not included).
2. **Interior alterations/interior tenant improvements.** Note: Publicly-accessible spaces (i.e. lobby, auditorium, or sanctuary) require preservation planner review.
3. **Regular maintenance and repair** to correct or repair deterioration, decay, or damage to the building.
4. **Window replacement** that meets the Department's *Window Replacement Standards* (does not include storefront window alterations).
5. **Garage work**, specifically, a new opening that meets the *Guidelines for Adding Garages and Curb Cuts*, and/or replacement of garage door in an existing opening.
6. **Deck, terrace construction, or fences** that are not visible from any immediately adjacent public right-of-way.
7. **Mechanical equipment installation** not visible from any immediately adjacent public right-of-way.
8. **Dormer installation** that meets the requirements for exemption from public notification under *Zoning Administrator Bulletin: Dormer Windows*.
9. **Additions** that are not visible from any immediately adjacent public right-of-way for 150' in each direction; does not extend vertically beyond the floor level of the top story of the structure or is only a single story in height; does not have a footprint that is more than 50% larger than that of the original building; and does not cause the removal of architectural significant roofing features.

NOTE:

Project Planner must check box below before proceeding.

Project is **not listed:**

GO TO STEP 5

Project **does not conform** to the scopes of work:

GO TO STEP 5

Project involves **4 or more** work descriptions:

GO TO STEP 5

Project involves **less than 4** work descriptions:

GO TO STEP 6

STEP 5 CEQA IMPACTS - ADVANCED HISTORICAL REVIEW (To be completed by Preservation Planner)

If condition applies, please initial.

1. Project involves a **Known Historical Resource (CEQA Category A)** as determined by Step 3 and conforms entirely to Scope of Work Descriptions listed in Step 4. (Please initial scopes of work in STEP 4 that apply.)

2. **Interior alterations to publicly-accessible spaces.**

3. **Window replacement** of original/historic windows that are not "in-kind" but are is consistent with existing historic character.

4. **Façade/storefront alterations** that do not remove, alter, or obscure character-defining features.

5. **Raising the building** in a manner that does not remove, alter, or obscure character-defining features.

6. **Restoration** based upon documented evidence of a building's historic condition, such as historic photographs, plans, physical evidence, or similar buildings.

7. **Addition(s)**, including mechanical equipment that are minimally visible from a public right of way and meets the *Secretary of the Interior's Standards for Rehabilitation*.

8. **Other work consistent with the Secretary of the Interior Standards for the Treatment of Historic Properties**
Specify: see PIR Form signed dated 4/27/13

- * 9. **Reclassification of property status to Category C**

 - a. Per Environmental Evaluation Evaluation, dated: _____
* Attach Historic Resource Evaluation Report
 - b. Other, please specify: _____

* Requires initial by Senior Preservation Planner / Preservation Coordinator

NOTE:
If ANY box is initialed in STEP 5, Preservation Planner MUST review & initial below.

Further Environmental Review Required.

Based on the information provided, the project requires an *Environmental Evaluation Application* to be submitted.

GO TO STEP 6

Preservation Planner Initials

Project Can Proceed With Categorical Exemption Review.

The project has been reviewed by the Preservation Planner and can proceed with categorical exemption review.

GO TO STEP 6

Preservation Planner Initials

STEP 6 CATEGORICAL EXEMPTION DETERMINATION (To be completed by Project Planner)

- Further Environmental Review Required.**
Proposed Project does not meet scopes of work in either:
(check all that apply)
- Step 2 (CEQA Impacts) or
 - Step 5 (Advanced Historical Review)

STOP!

Must file *Environmental Evaluation Application*.

No Further Environmental Review Required. Project is categorically exempt under CEQA.

Planner's Signature

4/27/13
Date

Rich Sore
Print Name

Once signed and dated, this document constitutes a categorical exemption pursuant to CEQA Guidelines and Chapter 31 of the Administrative Code.

Pereira, Monica

From: Beaupre, David
Sent: Tuesday, April 16, 2013 9:31 AM
To: Pereira, Monica
Subject: Copra Crane Questions- Response

Monica,

The Port would conduct removal activities in accordance with applicable regulatory permits and would cut or break the piles off at least one foot below the mudline. The Port will minimize sediment disturbance during removal, use a floating boom around the work area to contain and capture debris. Creosote treated piles and decking material will be placed on a barge and shipped to be processed and transferred to an appropriate upland disposal site. The repair crew will work diligently to prevent any material from dropping into the Bay during the course of the work, if any material falls into the Bay it will immediately be retrieved.

The new deck and support for the copra crane will be constructed from the waterside utilizing a barge and crane.

Please let me know if you have any other questions.

Thank you,

David Beaupre
Port of San Francisco
Planning and Development
415-274-0539
Fax 415-732-0409
sfport.com

PRESERVATION TEAM REVIEW FORM

Preservation Team Meeting Date: 4/15/2013	Date of Form Completion: 4/22/2013
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PROJECT INFORMATION:		
Planner:	Address:	
Rich Sucre	Pier 84 (Cupra Crane)	
Block/Lot:	Cross Streets:	
9900/084	Indiana and Tulare Streets	
CEQA Category:	Art. 10/11:	BPA/Case No.:
A		2013.0447E

PURPOSE OF REVIEW:	PROJECT DESCRIPTION:
<input checked="" type="radio"/> CEQA <input type="radio"/> Article 10/11 <input type="radio"/> Preliminary/PIC	<input type="radio"/> Alteration <input type="radio"/> Demo/New Construction

DATE OF PLANS UNDER REVIEW:	05/05/2003
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PROJECT ISSUES:	
<input type="checkbox"/>	Is the subject Property an eligible historic resource?
<input checked="" type="checkbox"/>	If so, are the proposed changes a significant impact?
Additional Notes:	
<p>- Constructed in 1965, the Pier 84 Copra Crane is a historic resource for CEQA purposes, as determined by the Central Waterfront Historic Resource Survey.</p> <p>- The proposed project includes reconstruction of the Copra Crane and construction of a new platform consisting of ten creosote-treated wood piles, pile caps, stringers and 1,100 sq ft of wood decking. The crane and platform would be reconstructed in its original location.</p>	

PRESERVATION TEAM REVIEW:				
Historic Resource per CEQA		<input checked="" type="radio"/> Yes	<input type="radio"/> No *	<input type="radio"/> N/A
Individual	Historic District/Context			
Property is individually eligible for inclusion in a California Register under one or more of the following Criteria: Criterion 1 - Event: <input type="radio"/> Yes <input type="radio"/> No Criterion 2 -Persons: <input type="radio"/> Yes <input type="radio"/> No Criterion 3 - Architecture: <input type="radio"/> Yes <input type="radio"/> No Criterion 4 - Info. Potential: <input type="radio"/> Yes <input type="radio"/> No Period of Significance: <input style="width: 150px;" type="text"/>	Property is eligible for inclusion in a California Register Historic District/Context under one or more of the following Criteria: Criterion 1 - Event: <input checked="" type="radio"/> Yes <input type="radio"/> No Criterion 2 -Persons: <input type="radio"/> Yes <input checked="" type="radio"/> No Criterion 3 - Architecture: <input type="radio"/> Yes <input checked="" type="radio"/> No Criterion 4 - Info. Potential: <input type="radio"/> Yes <input checked="" type="radio"/> No Period of Significance: <input style="width: 150px; border: 1px solid black;" type="text" value="1965"/> <input type="radio"/> Contributor <input type="radio"/> Non-Contributor			

Exhibit 2: City of San Francisco Planning Department CEQA Determination

Complies with the Secretary's Standards/Art 10/Art 11:	<input checked="" type="radio"/> Yes	<input type="radio"/> No	<input type="radio"/> N/A
CEQA Material Impairment:	<input type="radio"/> Yes	<input checked="" type="radio"/> No	
Needs More Information:	<input type="radio"/> Yes	<input checked="" type="radio"/> No	
Requires Design Revisions:	<input type="radio"/> Yes	<input checked="" type="radio"/> No	
Defer to Residential Design Team:	<input type="radio"/> Yes	<input checked="" type="radio"/> No	

* If No is selected for Historic Resource per CEQA, a signature from Senior Preservation Planner or Preservation Coordinator is required.

PRESERVATION TEAM COMMENTS:

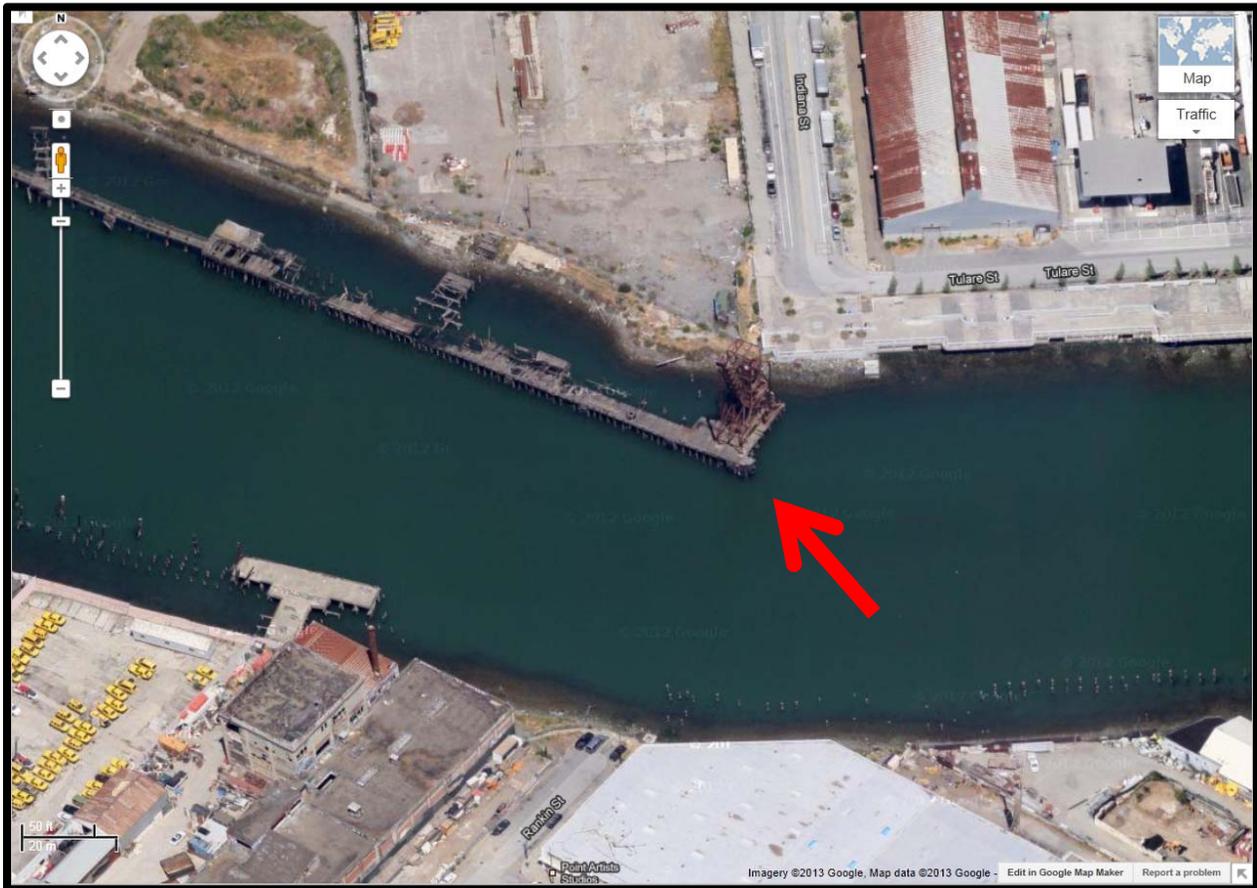
The Pier 84 Copra Crane is significant for its association with San Francisco's waterfront and labor history. It is the last surviving remnant of the former Cargill industrial plant, and is representative of the hand-operated machinery used by Longshoremen to off load material from cargo vessels. In 2012, the Copra Crane was disassembled and was stored off-site, due to imminent collapse of the timber wharf.

The proposed project would construct a new timber wharf and would reconstruct the Copra Crane in its original location. The reconstruction of the Copra Crane would be guided by detailed architectural and engineering drawings, and would meet the Secretary of the Interior's Standards for Reconstruction. The new timber wharf would match the original timber wharf in location, design, size and appearance. Ultimately, the reconstructed Copra Crane would serve as an monument to recognize and interpret the history of the Copra Crane and its contribution to San Francisco's waterfront. The proposed project is sponsored by the Port of San Francisco with the Copra Crane Labor Landmark Association (CCLLA).

Overall, the proposed project would not have a significant adverse impact to any on-site or off-site historic resources, since the proposed project would meet the Secretary of the Interior's Standards for Reconstruction.

Signature of a Senior Preservation Planner / Preservation Coordinator:	Date:
	4 - 22 - 2013

IMAGES



Pier 84, Copra Crane
(Source: Google Maps, Accessed April 22, 2013)

State of California — The Resources Agency DEPARTMENT OF PARKS AND RECREATION PRIMARY RECORD	Primary # _____ HRI # _____ Trinomial _____ NRHP Status Code _____
	Other Listings _____ Review Code _____ Reviewer _____ Date _____

Page 1 of 2 Resource name(s) or number (assigned by recorder) Pier 84 and Copra crane

P1. Other Identifier: 133; Pier 84 Cargill Copra Plant

***P2. Location:** Not for Publication Unrestricted

***a. County** San Francisco

***b. USGS 7.5' Quad** San Francisco South, CA **Date** 1995

***c. Address** Indiana and Tulare Streets at Islais Creek Channel

City San Francisco

Zip 94124

***e. Other Locational Data:** Assessor's Parcel Number Block: 9900 Lot: 84

***P3a. Description:** (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries.)

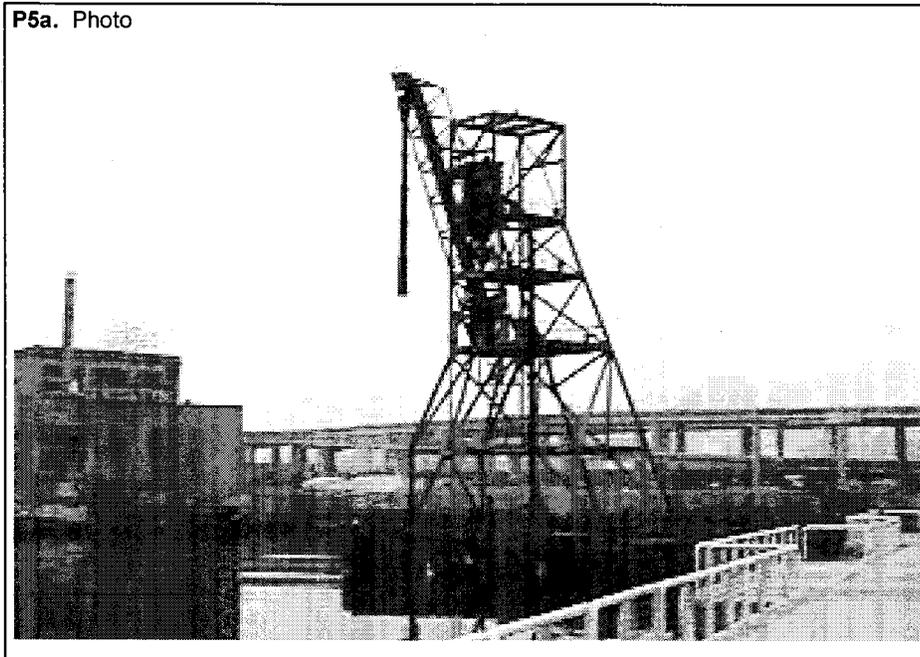
Pier 84 is a single level wooden wharf set on wood pilings located along the northwest shore of the Islais Creek Channel. The wharf is located some fifty feet from the shoreline and was accessed by short bridges, no longer standing. Adjacent buildings have also been demolished, c. 1998.

The copra crane is a large (approximately 50-foot tall) structural steel loading crane with a broad base set onto the deck of the pier. A long arm and descending "trunk" project from the midsection of the crane. The crane has been tooled to unload copra, or dried coconut meat, which was processed for vegetable oil in the now demolished plant on the adjacent shore.

***P3b. Resource Attributes:** (list attributes and codes) HP 11 Engineering Structure; AH 13 Wharves

***P4. Resources Present:** Building Structure Object Site District Element of District Other

P5a. Photo



P5b. Photo: (view and date)

View from Tulare Street looking southwest
11-30-2000

***P6. Date Constructed/Age and Sources:** historic

Pier: 1948 Port Files

Crane: 1965 Port Files

***P7. Owner and Address:**

Port of San Francisco

Ferry Building

San Francisco, Ca 94111

***P8. Recorded by:**

Planning Department

City & County of San Francisco

1660 Mission Street, 5th Floor

San Francisco, CA 94103

***P9. Date Recorded:** 01-19-2001

***P10. Survey Type:**

Intensive

***P11. Report Citation:** (Cite survey (1990.348L) "Pier 84 with Loading

report and other sources, or enter "none") San Francisco Landmarks Case Report, June 1, 1990 "Tower"; DPR 523 10/24/94

***Attachments:** None Location Map Sketch Map Continuation Sheet Building, Structure, and Object Record

Archaeological Record District Record Linear Feature Record Milling Station Record Rock Art Record

Artifact Record Photograph Record Other

State of California — The Resources Agency **Primary #**
DEPARTMENT OF PARKS AND RECREATION **HRI#**
BUILDING, STRUCTURE, AND OBJECT RECORD

Page 2 of 2

*NRHP Status Code 4S1

*Resource Name or # Pier 84 and copra crane

- B1. Historic name: Cargill Inc. Copra loading crane
- B2. Common name: Copra loading crane
- B3. Original Use: Copra loading crane
- B4. Present use: None

*B5. **Architectural Style:** N/A

*B6. **Construction History:** (Construction date, alterations, and date of alterations)
 Pier constructed in 1948. Crane constructed in 1965.

*B7. **Moved?** No Yes Unknown **Date:** _____ **Original Location:** _____

*B8. **Related Features:**

Shed and pump house, both post-1964. Rail spurs. Office on Cesar Chavez Street.

B9a. Architect: Unknown

b. Builder: Unknown

*B10. **Significance: Theme** Commercial Development **Area** San Francisco's Central Waterfront

Period of Significance 1854-1948 **Property Type** Industrial **Applicable Criteria** A

(Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity)

This area does not appear on any maps before 1920 because it was an open creek bed. By 1928, work had begun on the Islais Creek Reclamation project, which entailed creating 281 acres of fill for industrial expansion and new factory sites and dredging 6,000,000 cubic yards of fill to create the present channel. This parcel is listed in the 1935 block book with the Western Pacific Rail Road Company as the owner of the entire block. In 1948, the Port of San Francisco constructed Pier 84. By 1948, a plant and refinery for coconut oil was constructed by Cargill Inc., and in 1956, the administration building was constructed with general offices, an oil manager office, a grain manager office, a trading room and a sample room. The loading crane - copra crane was erected in 1965 by/for Granex Corp., a copra processing plant owned and operated by Philippine nationals. It was used until 1974 when the copra processing plant closed. City Directories list Cargill Inc. at this address until 1980 and Granex Corporation, refiners of oil/copra processing from 1981 until 1990. Pan Pacific Commodities, dealers of crude oil, were also listed at this address from 1981-1983.

The crane retains integrity of location, design, workmanship, materials, and association. It is such a striking structure that, although almost all of the buildings from the plant have been demolished, it still possesses integrity of setting and feeling.

Although this crane was erected less than 50 years ago, it is significant at the local level because of its connection to the Central Waterfront's and San Francisco's labor history as it is the last remaining piece of machinery on the Port of San Francisco hand-operated by longshoremen working bulk cargo. It also represents the ties of San Francisco's economy with those of the South Pacific Islands. In the 1960s, copra imports to San Francisco were valued at about \$18 million annually. This resource may become eligible for separate listing in the National Register when the property becomes old enough to meet the Register's 50-year requirement. Additional research may find it as the only remaining property representative of the copra processing industry in San Francisco. It is significant under Criterion A: Resources that are associated with events that have made a significant contribution to the broad patterns of our history.

B11. Additional Resource Attributes: (List attributes and codes)

*B12. **References:**

Building Permit #188857; Port of San Francisco Historic Resources Data Base; "Save the Copra Crane" brochure; Copra Crane Labor Landmark Association; Islais Creek Human History Outline

B13. Remarks:

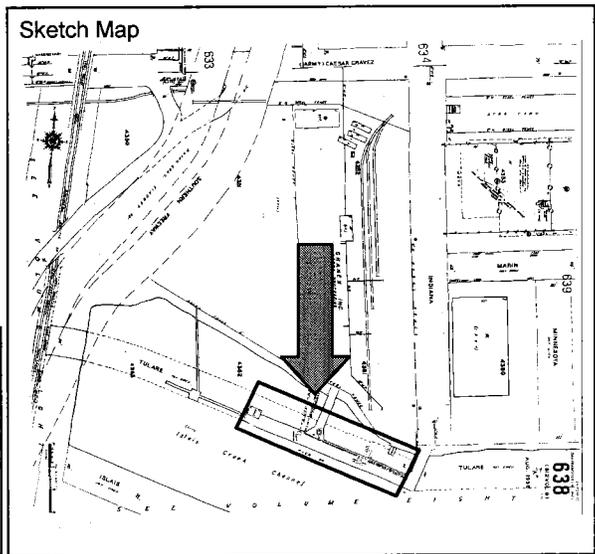
*B14. **Evaluator:**

Tim Kelley, historian, Central Waterfront Survey Advisory Committee

*Date of Evaluation:

July 20, 2001

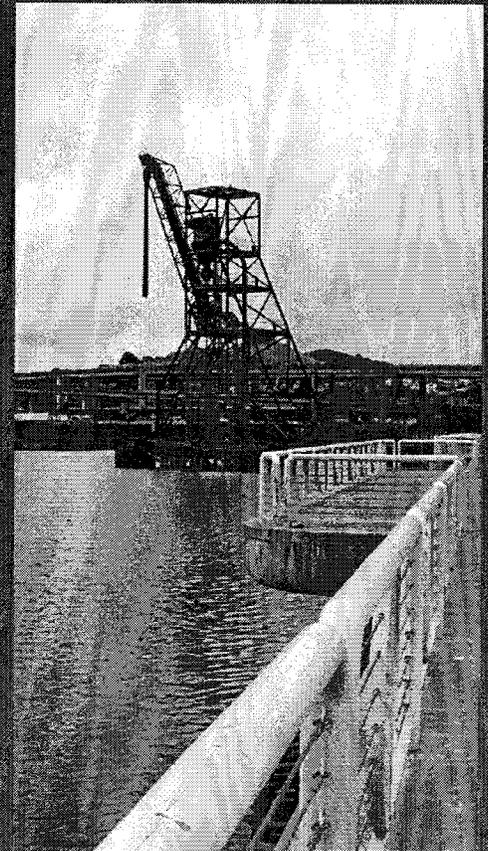
(This space reserved for official comments.)



Copra Crane until 2012

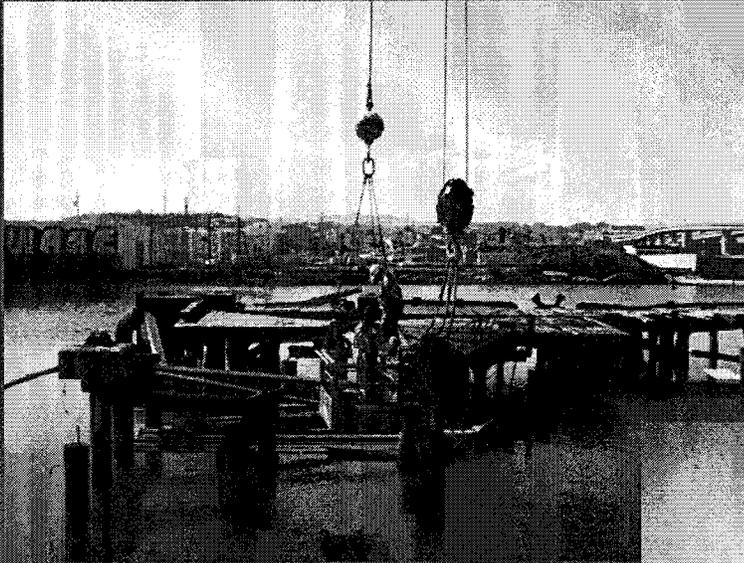


Copra Crane, looking east from wharf, (1980's)
at Islais Creek



Copra Crane, looking west from PUC
promenade (2010) at Islais Creek

Copra Crane former and future site



Above: Piles that once supported Copra Crane, looking south at Terminus of Indian Street at Islais Creek

Below: Pile supported wharf area adjacent to former Copra Crane Platform, Looking west at terminus of Indiana Street



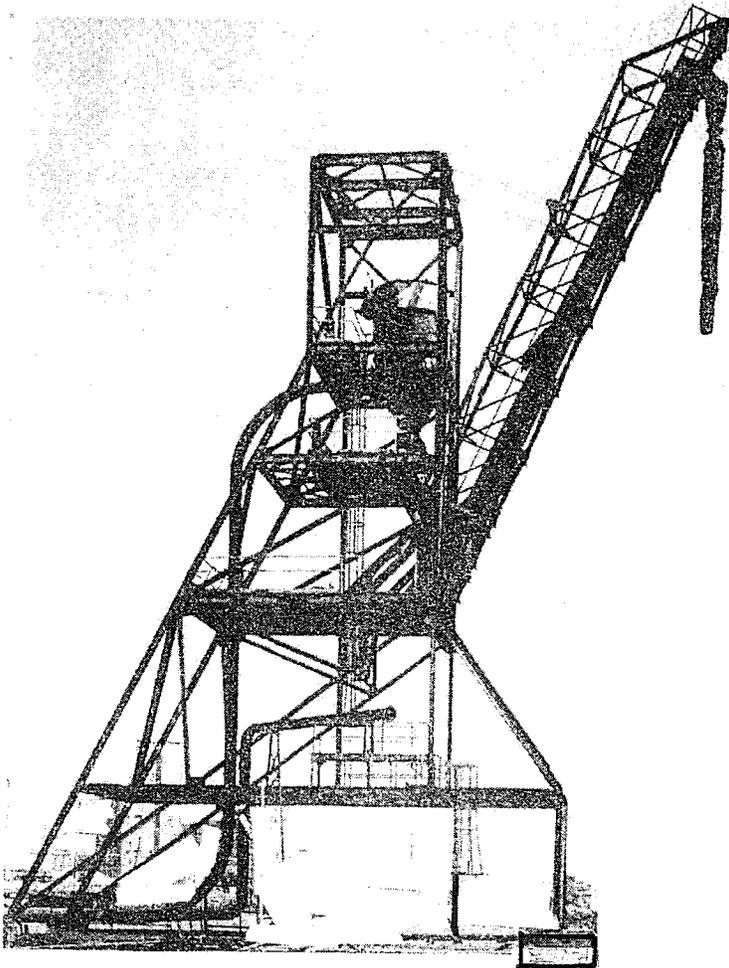


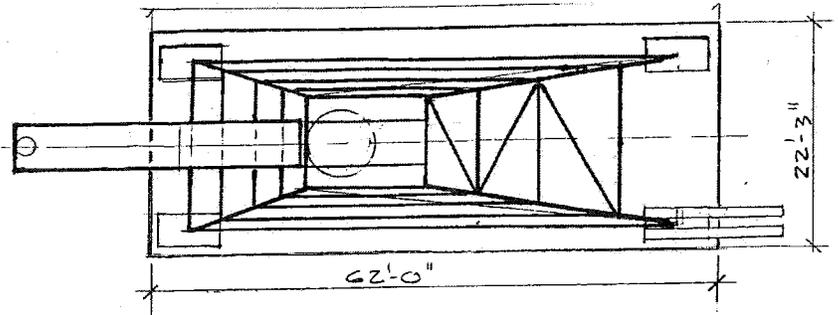
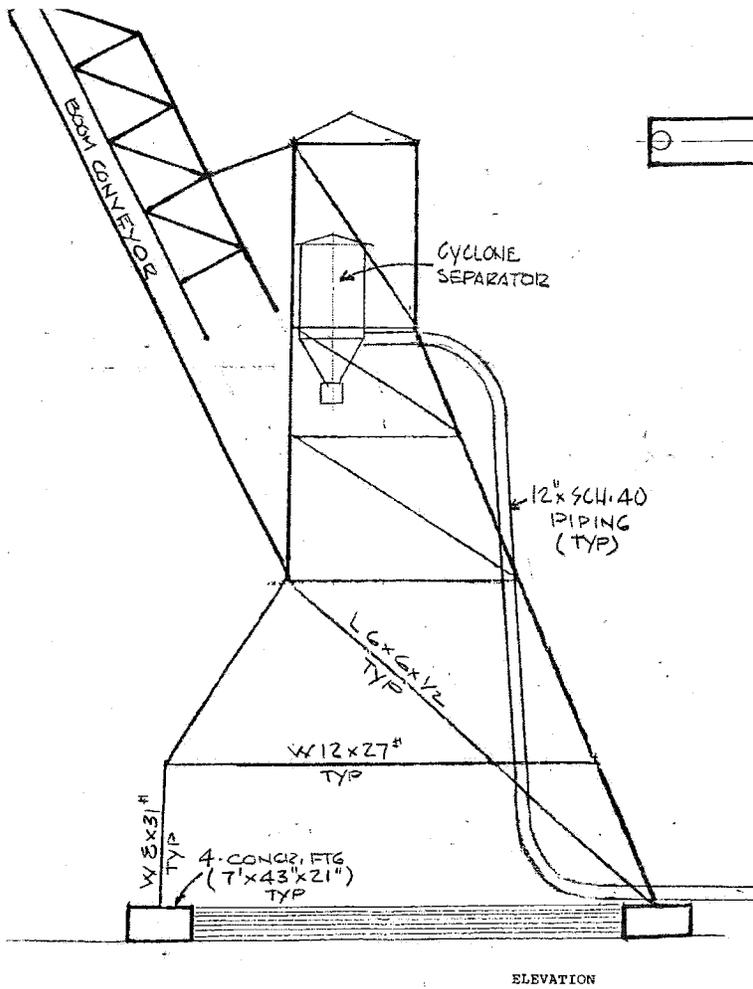
TABLE OF CONTENTS:

SHEET NO.	CONTENT
1.	TITLE SHEET & TABLE OF CONTENTS.
2.	COPRA CRANE; GENERAL ARRANGEMENT, PLAN & ELEVATION.
3.	NEW COPRA CRANE PLATFORM; STRUCTURAL ARRANGEMENT.
4.	NEW COPRA CRANE PLATFORM; CORNER CONSTRUCTION DETAILS.
5.	PILE CONSTRUCTION.



	HARBOR DESIGN ENGINEERS 365 TENNESSEE AVE. • MILL VALLEY 94941 PHONE & FAX 415/388-9362	
	CARGILL COPRA CRANE LABOR MONUMENT PIER 84/ISLAÏS CREEK	
DRAWN BY: R. MCCARDELL	APPVD:	SCALE:
DRAWING NO: C0305-001	REV:	DATE: 5/5/03

SHEET NO. 1 OF 5

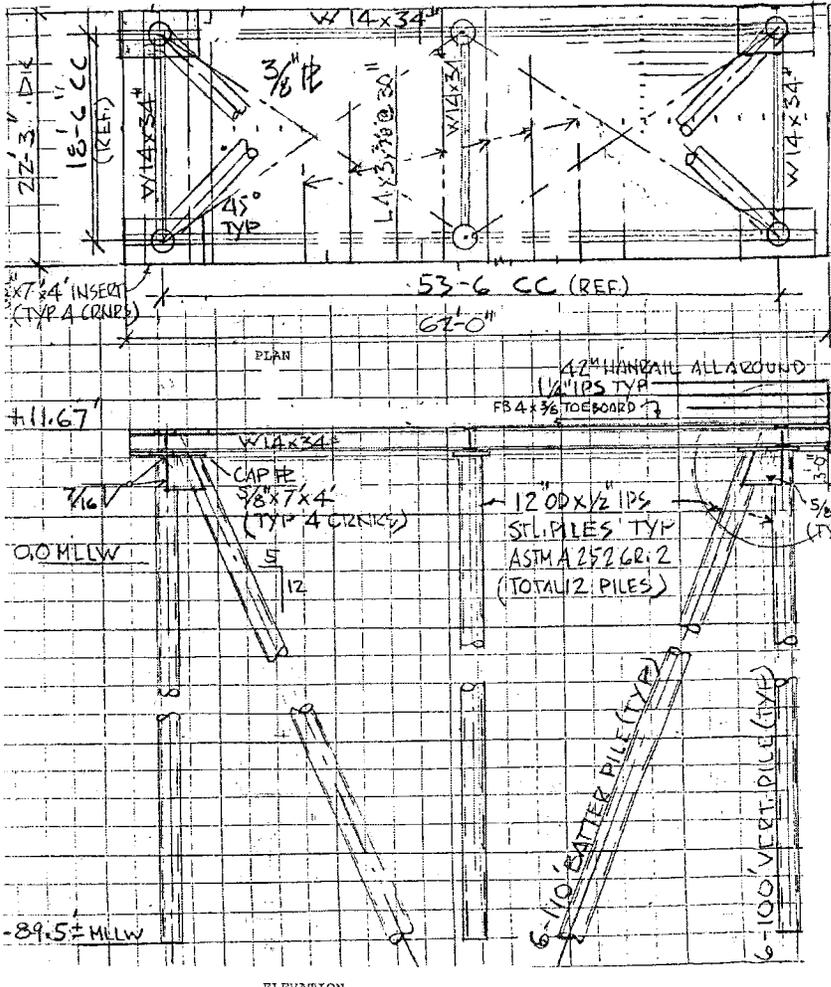


NOTE:

ALL STRUCTURE SHOWN ON THIS SHEET REFLECTS THE EXISTING AS-BUILT CONDITION.



 <p>HARBOR DESIGN ENGINEERS 366 TENNESSEE AVE. • MILL VALLEY 94941 PHONE & FAX 415/388-9062</p>		
<p>CIVIL • MARINE • MECHANICAL</p> <p>CARGILL COPRA CRANE LABOR MONUMENT</p> <p>GENERAL ARRANGEMENT</p>		
<p>DRAWN BY: R.MCCARDELL</p>	<p>APPVD:</p>	<p>SCALE: 1"=10'</p>
<p>DRAWING NO: C0305-001</p>	<p>REV:</p>	<p>DATE:</p>

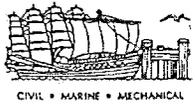


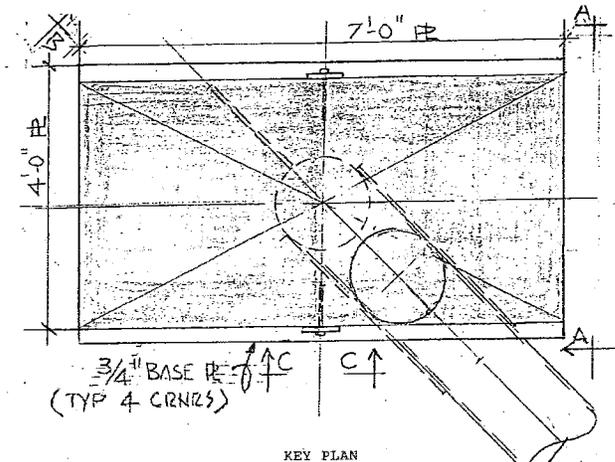
GENERAL NOTES:

1. ALL STEEL PLATE & SHAPES , ASTM A-36, OR BETTER; BOLTS, GR. 2 GALVANIZED. (SEE SHEET 2 FOR PILING MATERIALS & DETAILS.)
2. ALL WELDING, MATERIALS & WORKMANSHIP SHALL MEET CURRENT AWS SPECIFICATIONS; ALL BUTT WELDS SHALL BE FULL PENETRATION WITH REINFORCEMENT; ALL FILLET WELDS SHALL BE DOUBLE CONTINUOUS SIZED 1/16" LESS THAN THE THINNER OF THE MATERIALS BEING JOINED DECK BEAMS MAY BE EITHER CHAINED OR STAGGERED INTERMITTENT WELDED WITH 6" DBL. CONTINUOUS WELDS AT EACH END.
3. THE ENTIRE PLATFORM SHALL BE PROVIDED WITH A MULTI TIER HANDRAIL INCLUDING A FB 4"x3/8" TOEBOARD TO MEET UBC STANDARDS; AN OPENING AT MID-LENGTH ON THE EAST FACE SHALL BE PROVIDED & A VERTICAL LADDER SHALL BE INSTALLED FOR PERMANENT ACCESS TO THE PLATFORM. THE LADDER CONSTRUCTION SHALL BE STEEL GALVANIZED WITH NON-SKID RUNGS. A SAFETY CHAIN & HASP ALL CRS OR GALVANIZED SHALL BE PROVIDED & INSTALLED AT THE OPENING IN THE HANDRAIL
4. AFTER COMPLETION OF ALL WELDING, ALL NEW STEEL TO BE PREPARED PER SSPC-SP-6, CLEANED THOROUGHLY & COATED WITH A WATERBORNE ZINC SILICATE, DIMETCOTE 302H. ANY DISTURBED STEEL SHALL BE DISC GROUND TO BARE METAL & COATED WITH THE SAME PRODUCT; WELD AREAS TO BE REPAIRED WITH COMPATIBLE APPROVED MATERIALS.
5. DO NOT SCALE DRAWING; CHECK WITH THE ENGINEER IN CASE OF ANY DOUBT.

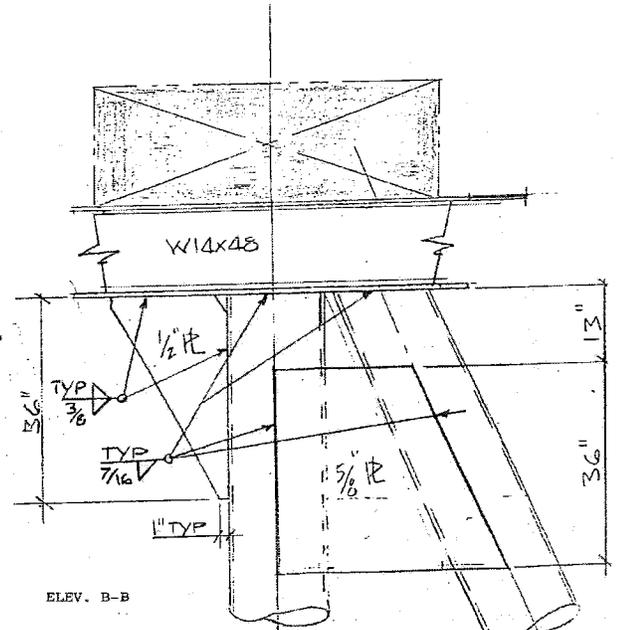
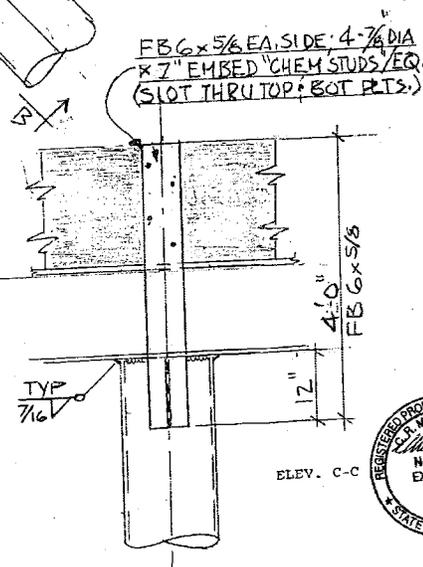
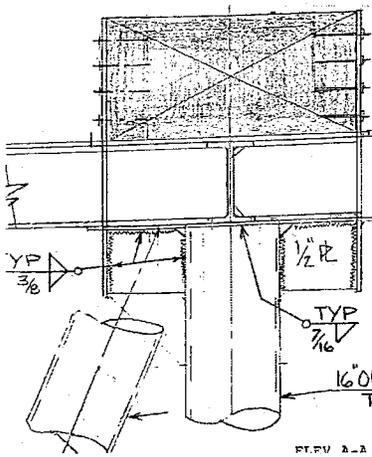
SEE SHT. 4 FOR CORNER/PILE CONSTRUCTION DETAILS.



 <p>HARBOR DESIGN ENGINEERS 366 TENNESSEE AVE. • MILL VALLEY 94941 PHONE & FAX 415/388-9362</p> <p>CARGILL COPRA CRANE LABOR MONUMENT</p> <p>CIVIL • MARINE • MECHANICAL</p> <p>PLATFORM STRUCTURAL ARRANGEMENT</p>		
DRAWN BY: R. MCCARDELL	APPVD:	SCALE: 1/8" = 1' - 0
DRAWING NO: C0305-001	REV:	DATE:



- GENERAL NOTES:
1. CONCRETE FOOTINGS ARE SHOWN SHADED.
 2. BEFORE SETTING THE COPRA CRANE UPON THE NEW PLATFORM, THE STEEL SURFACES IN WAY OF THE CONCRETE FOOTINGS SHALL BE TREATED WITH "CHEMCO" BONDING LIQUID LNL OR EQ.; THE CONCRETE FOOTING SURFACES SHALL ALSO BE COATED WITH THE SAME PRODUCT TO REDUCE PERMEABILITY TO MOISTURE PENETRATION.

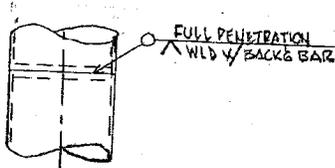


HARBOR DESIGN ENGINEERS
366 TENNESSEE AVE. • MILL VALLEY 94941
PHONE & FAX 415/388-9362

CARGILL COPRA CRANE LABOR MONUMENT

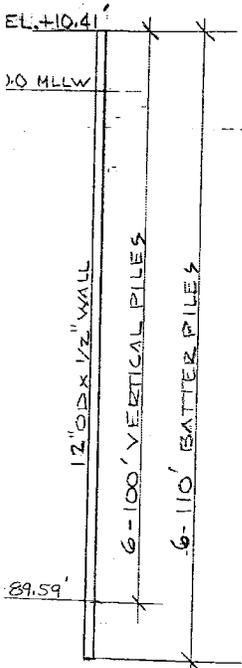
CIVIL • MARINE • MECHANICAL

DRAWN BY: R. MCCARDELL	APPVD:	SCALE: 3/4"=1'-0"
DRAWING NO: C0305-001	REV:	DATE:



TYPICAL PILE SPLICE

1" = 2'



ELEVATION

PILE SPECIFICATIONS & NOTES:

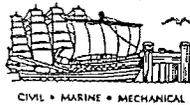
1. ALL PILES TO MEET ASTM A-252 GR. 2 OR 3.
2. PILE BUTT WELDS SHALL BE FULL PENETRATION WITH BACKING BARS.
3. PILE TIPS SHALL BE MACHINE CUT & BEVELED 30 DEGREES.
4. PILE DRIVING

TOLERANCES- VERTICAL CUT OFF @ +10.41' MLLW.
 HORIZ. +/- 3" FROM PLANNED LOCATION.
 PLUMB 1 1/2" IN 10'

AFTER PILES ARE DRIVEN TO FINAL POSITION, MATERIAL WITHIN THE ANNULAR SPACE SHALL BE REMOVED & THE BOTTOM 10' FILLED WITH CONCRETE; THE REMAINING SPACE SHALL BE FILLED WITH SAND/GRAVEL AGGREGATE, 3/4" MAX. GRAIN SIZE.

5. COATING-
 PREPARATION- ALL UNCOATED SURFACES TO BE CLEANED TO SSPC-SP6 COMMERCIAL BLAST. APPLY PAINT IMMEDIATELY AFTER SURFACE PREPARATION.
 APPLICATION- APPLY 1 COAT DIMETCOTE 302H 4 MILS DFT. ALLOW 4 HRS. DRY TIME @50 DEGREES F. BEFORE OVERCOATING.
 APPLY 1 COAT AMERON PSX700, 4-6 MILS DFT
6. ALL WELDING TO MEET CURRENT AWS SPECIFICATIONS.



 HARBOR DESIGN ENGINEERS 366 TENNESSEE AVE. • MILL VALLEY 94941 PHONE & FAX 415/388-9382		
CARGILL COPRA CRANE LABOR MONUMENT PILE CONSTRUCTION		
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DRAWING NO: C0305-001	REV:	DATE:

SHEET NO. 5 OF 5.